

APPENDIX

C

SUMMARY OF
COMMENTS
RECEIVED

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
<i>Notice of Study Commencement (January 26, 2017)</i>					
Indigenous Communities					
1	February 15, 2017 / Fax	Alderville First Nation	<ul style="list-style-type: none"> Expressed interest in the archaeological assessments/environmental assessments 	<ul style="list-style-type: none"> No response was required 	No
Local Elected Officials					
2	February 1, 2017 / Email	Councillor David Chernushenko	<ul style="list-style-type: none"> Suggested ameliorated pedestrian and cycling experience under the new underpasses would improve accessibility for all road and modal users. Suggested that wider sidewalks and space for dedicated cycle lanes would also be beneficial. 	<ul style="list-style-type: none"> The following is in response to your email sent on behalf of David Chernushenko, City of Ottawa Capital Ward councillor, regarding an enhanced pedestrian and cycling experience under the new overpasses as part of the Ministry's Detailed Design and Class Environmental Assessment (EA) Study for the replacement and rehabilitation of 10 bridges (sites) on Highway 417 between Preston Street and Percy Street. The additional improvements you have identified would fall under the City's responsibility. The City would be the proponent of any changes to the cross-section of the streets that pass beneath the Queensway within the space and envelope available for municipal needs, and these considerations would be at the cost of the city. The ability to widen bridges for replacement structures was considered as part of the completed Preliminary Design and Environmental Assessment Study for the Rehabilitation/Replacement of Ottawa Queensway mid-town bridges from Holland Avenue to O'Connor Street, but were not carried forward in the recommended plan, as doing so increases the complexity and risk associated with the replacement options, and significantly increases the capital costs to the Ministry's bridge projects. Should the City wish to fund improved cycling and pedestrian facilities under the new overpasses, please contact the undersigned at (613) 545-4723 or by e-mail at ken.rogers@ontario.ca. 	MTO PM responded by email February 6, 2017
External Agencies					
3	January 26, 2017 / Email & Comment Form	National Research Council (NRCan)	<ul style="list-style-type: none"> Noted that they are working with MTO to provide on-site construction staging for the Rochester Bridge location 	<ul style="list-style-type: none"> No response required 	No
4	January 26, 2017 / Fax & Comment Form	City of Ottawa	<ul style="list-style-type: none"> Confirmed participation in project 	<ul style="list-style-type: none"> No response required 	No
5	January 27, 2017 / Email	National Capital Commission (NCC)	<ul style="list-style-type: none"> Noted that potential matters of Capital/NCC interest include the noise barrier near Island Park Drive (Capital Parkway) Highway 417 is a 'Capita Arrival' in Plan for Canada's Capita (1999) 	<ul style="list-style-type: none"> No response required 	No
6	January 31, 2017 / Email & Comment Form	Ottawa Student Transportation Authority (OSTA)	<ul style="list-style-type: none"> Requested to be added to the contact list to receive updates and to respond to answer questions regarding student transportation in the area (English Boards) 	<ul style="list-style-type: none"> No response required Contact was added to the contact list 	No
7	February 10, 2017 / Email & Comment Form	Ministry of Natural Resources and Forestry	<ul style="list-style-type: none"> Noted that a Natural heritage / SAR Screening is to follow. 	<ul style="list-style-type: none"> No response required 	No
Businesses					
8	January 26, 2017 / Phone	Preston Street BIA	<ul style="list-style-type: none"> Commented that they thought the Preston Street Bridge Rehabilitation was completed as there was work on the structure Fall 2016. Due to the work, the BIA had removed their mural (at considerable cost) and want to verify that no further work is planned. Inquired about the timing for the Rochester and Booth Street Bridge replacements. Noted that during the Preliminary Design, it was identified that the parking lot at Beech and Rochester would be utilized for staging. The Preston Street BIA has concerns over the loss of parking in Little Italy and are strongly opposed to this. Inquired about the key map and the noise barriers that were being replaced. 	<ul style="list-style-type: none"> They were informed that the work undertaken under the Minor Capital Contract was maintenance work and short term repairs and that the full bridge rehabilitation was yet to take place. This project was to do that. The BIA asked about possible compensation to help with the removal and replacement of the mural and was directed to ask that question of MTO. The timing of the replacements is tentatively scheduled for construction 2019-2022, subject to regional priorities and environmental approvals. Final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be established during the detailed design with consideration for traffic operations, construction staging, operational constraints, and constructability. Informed the BIA that the Preston Street Bridge is now a rapid rehabilitation not replacement. Considering this the preferred staging area for the Rochester and Booth Street bridges is the Government Parking area off of Orangeville Street and potentially Orangeville Street itself. Explained that this notice only contained the noise barriers that were being designed under this project however there are additional noise barriers identified as part of the Noise Barrier Retro-fit project which will be combined with this 	WSP provided a response by phone on January 26, 2017.

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				project for construction. The project website provides links to other websites that deal directly with the other noise barriers. Also, MTO recently undertook some additional work and identified additional retro-fit locations but these are not included within this project.	
9	February 2, 2017 / Email & Comment Form	Arnon Corporation	<ul style="list-style-type: none"> Requested to be advised of the chosen bridge staging area for the Preston and Rochester Bridge Replacements 	<p>Thank you for your comments received on February 2, 2017 regarding the Highway 417 Bridge Replacements and Operational Improvements project (Highway 417 Midtown Bridges). You requested to be informed of the chosen bridge staging areas for the Preston Street Bridge and Rochester Street Bridge replacements.</p> <p>The Preston Street Bridge and Rochester Street Bridge staging areas are now known and were developed in consultation with stakeholders and the final Recommended Plan for the detail design.</p> <p>The Preston Street Bridges will be staged at the Ottawa District School Board soccer field located at the intersection of Gladstone Avenue and Preston Street. The Rochester Street Bridges will be staged within a portion of a vacant Ottawa Community Housing site (planned to undergo redevelopment) at 79-83 Raymond Street between Raymond Street, Rochester Street and Booth Street.</p> <p>An online Public Information Centre (PIC) is currently being held for the Highway 417 Midtown Bridges Project at www.highway417-midtownbridgesandimprovements.com. The locations of each of the staging areas, as well as construction schedule and traffic impacts can be viewed online as part of the online PIC material.</p> <p>Should you require additional information, please feel free to contact me.</p>	WSP provided a response by emailed letter on February 24, 2020.
10	February 8, 2017 / Email & Comment Form	Dominos	<ul style="list-style-type: none"> Requested additional information about closures on Preston Street, and detailed information on impacts to their business 	<p>Thank you for your comments received on February 8, 2017 regarding the Highway 417 Bridge Replacements and Operational Improvements project (Highway 417 Midtown Bridges). You requested additional information regarding closures of Preston Street during construction and how it may affect your business.</p> <p>Preston Street closures are now known and were developed in consultation with stakeholders and the final Recommended Plan for the detail design.</p> <p>Preston Street will be closed between St. Anthony Street and Young Street for approximately 3 weeks (mid July – August) for preparatory works to accommodate the rapid replacement of the bridge, anticipated during 2024. Access to 300 Preston Street will be maintained during this time.</p> <p>Please note that the construction schedule is subject to regional priorities and environmental approvals. As well, given the scope and magnitude of the project, the work sequencing may vary based on the Contractor's schedule of operations. The Contractor will be required to notify adjacent properties a minimum of one week in advance of construction start with a more detailed construction schedule.</p> <p>An online Public Information Centre (PIC) is currently being held for the Highway 417 Midtown Bridges Project at www.highway417-midtownbridgesandimprovements.com. Further information regarding staging areas, construction schedule and traffic impacts can be viewed online as part of the online PIC material.</p> <p>Should you require additional information, please feel free to contact me.</p>	WSP provided a response by emailed letter on February 24, 2020.
11	February 16, 2017 / Email & Comment Form	Sakto Corporation	<ul style="list-style-type: none"> Confirmed participation in project 	<ul style="list-style-type: none"> No response required 	No
12	February 23, 2017 / Email	Taggart Realty Management	<ul style="list-style-type: none"> Stated that they were property owners of a property in the study area near the Bronson Avenue Highway 417 eastbound off-ramp. Noted that they have reviewed the Study Commencement Letter and materials available on the project website and are interested in knowing how the proposed changes to the Bronson interchange will impact their property and the timing of construction. 	<p>WSP provided a response by email on March 16, 2017:</p> <ul style="list-style-type: none"> We want to thank you for your inquiry. We are reviewing the proposed design with respect to any impacts to your property and will provide a more detailed response when the review has been completed and the design has advanced to a stage where we can determine exact impacts. We acknowledge that your property is extremely close to the intersection and will strive to minimize any impacts to your property. Once the design has progressed I will be in a better position to provide more detailed information. <p>WSP provided a response by email on October 23, 2017:</p> <ul style="list-style-type: none"> We have reviewed our design further and have identified some temporary and permanent impacts and would like to arrange an on-site meeting to review & discuss the impacts with you. Please let me know when you would like to meet so that I can arrange for MTO to also be present. 	WSP provided a response by email on March 16, 2017 and on October 23, 2017.

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
Public					
13	April 20, 2016	Public	<ul style="list-style-type: none"> Inquired when the noise barriers on Highway 417 westbound between Parkdale and Carling will be replaced 	<ul style="list-style-type: none"> Thank you for your e-mail April 20, 2016 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). The Highway 417, Bridge Replacement/Rehabilitation and Operational Improvements project entails the replacement of the existing steel noise barriers from Island Park Drive to Lyon and has been grouped with the Ministry's bridge replacement /rehabilitation project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor. Construction is tentatively scheduled for 2019-2022, subject to regional priorities and environmental approvals. Given the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability. You may follow the detailed design at the following website: http://www.highway417-midtownbridgesandimprovements.com 	WSP provided a response by email on May 25, 2017.
14	November 8, 2016 / Phone	Public	<ul style="list-style-type: none"> Called about the replacement of the noise barriers between Island Park and Parkdale Ave. Key concern is when – the sooner the better Requested to be added to the Study contact list 	<ul style="list-style-type: none"> Advised her of the noted project and start of Detail Design 	MTO provided a response by phone on November 8, 2016.
15	January 25, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Inquired about the timing of the replacement of the noise barrier on the north side of Highway 417 between Island Park and Parkdale Expressed support about the project 	<ul style="list-style-type: none"> Thank you for your e-mail January 25, 2017 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). The replacement of the existing steel noise barriers from Island Park Drive to Lyon has been grouped with the Ministry's bridge replacement /rehabilitation project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor. Construction is tentatively scheduled for 2019-2022, subject to regional priorities and environmental approvals. Given the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability. 	WSP provided a response by email on March 21, 2017.
16	January 26, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Requested to be added to the contact list of future updates and information 	<ul style="list-style-type: none"> No response required 	No
17	January 27, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Inquired about which year Raymond Street will be affected by the project 	<ul style="list-style-type: none"> Thank you for your email dated January 27th, 2017 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). Construction is tentatively scheduled for 2019-2022, subject to regional priorities and environmental approvals. Given the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability. 	WSP provided a response by email on March 21, 2017.
18	January 29, 2017 / Email	Public	<ul style="list-style-type: none"> Stated that they have given thought to the noise barriers along Highway 417 and throughout Ottawa and stated that throughout the world, cities have constructed attractive precast concrete sound walls with attractive scenic images. Noted that the noise barriers in Ottawa are a disgrace to the Nation's Capital City. Stated that the examples they have looked at are not painted, just imprinted onto the concrete. Noted that there are examples along the Laurentien autoroute near Montreal that have survived winter. Suggested that the concrete could be sea wall concrete, resistant to salt. Provided examples of local festivals and places of interest as proposed imagery for the sound barriers 	<ul style="list-style-type: none"> Thank you for your e-mail of January 29, 2017 regarding the existing conditions and the replacement of noise barriers along Highway 417 Ottawa Queensway. Many of the noise barriers constructed in Ottawa were among the first in the province; and in 2007 the Ministry completed a comprehensive review of the Ottawa Queensway and identified a long-term plan to rehabilitate and upgrade infrastructure, including considerations for the replacement of existing metal noise barriers and provisions for new sections of noise barriers along the Queensway corridor. In keeping with the above, in 2011 the Ministry in consultation with the City of Ottawa, National Capital Commission, and Park Canada developed Context Sensitive Design (CSD) concepts for the Highway 417 Queensway from Highway 416 to Walkley Road. The CSD study identified a suite of context sensitive design elements to be implemented on future projects as a consistent approach throughout the corridor for lighting, bridges (overpass and underpass), landscaping and noise barriers. In February 2012 as part of the Highway 417 expansion from Nicholas Street to Ottawa Road 174 the Ministry 	MTO provided a response by email on February 6, 2017.

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				<p>presented the Context Sensitive Designs to the public for review. As per the accepted CSD elements, new noise barrier façades (durisol concrete barriers) include both colour and texture in keeping with the architectural context (styles, colours and surfaces) of buildings in downtown Ottawa, and with consideration for clear noise barrier panels at structural bridge overpasses to provide vistas of the City. While there were considerations for murals and landscapes, these elements were precluded on the basis of highway safety as such elements can be considered a highway distraction.</p> <ul style="list-style-type: none"> To date the Ministry has received positive feedback on the CSD elements that have been implemented. Please refer to the attached photo of a recently completed Ministry contract illustrating some of the context sensitive design elements to be considered on future Ministry projects within the Ottawa Queensway corridor. 	
19	February 4, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Requested to be added to the mailing list Stated that as a resident, they welcome the project but want to ensure that it will result in high quality upgrades and not just a replacement of existing infrastructure. Noted that the south side sound barrier west of Champagne Avenue is flimsy and too low to be effective. Stated that they have observed snowplows throwing snow and ice over the barrier, hitting their come and parked cars. Stated that a more solid barrier of double the height would be an improvement and would be more effective. Expressed concerns about the impact that construction activities will have on local residents. Stated that recent highway resurfacing work was completed after 11 p.m. during week night which resulted in strong vibration and bright flood lights. Stated that limiting work to daytime hours and compressing it into a shorter time period would be more respectful of residential areas. 	<ul style="list-style-type: none"> Indicated that no response is required Contact was added to the contact list 	No
20	February 16, 2017 / Email	Public	<ul style="list-style-type: none"> Stated that they live beside Highway 417, near the noise barrier on the north side of Highway 417 and Island Park. Noted that the vehicles using the Island Park off-ramp are a nuisance to the adjacent homes. Suggested that the design of the noise barrier be extended by 100 ft farther west. 	<ul style="list-style-type: none"> Thank you for your email dated February 16th, 2017 regarding the noise barriers at the Highway 417 (Queensway) westbound off-ramp to Island Park Drive. The existing noise barrier in question will be reviewed as part of the detail design that has recently commenced for the replacement/ rehabilitation of 10 bridges (5 sites) on Highway 417 between Preston Street and Percy Street, as well as operational improvements on Highway 417 from Island Park Drive to east of Lyon Street, in the City of Ottawa (GWP 4173-15-00). Your suggestion to extend the existing noise barrier along the westbound off-ramp at Island Park Drive will be reviewed as part of a noise study assessment that will take into account the specific characteristics of the study area. Your contact information will be added to mailing list for the detail design study, whereby you will receive updated information as the project evolves. You may also follow the detail design project at the following website: /www.highway417-midtownbridgesandimprovements.com 	MTO provided a response by email on March 21, 2017.
21	February 22, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Stated that they live along Young Street within the study area. Provided a suggestion that the noise barrier be made as tall as possible and inquired if the height of the new noise barrier will be taller than what currently exists. Inquired about the materials of the noise barrier and stated that they do not want a clear wall, similar to other observed along the Highway 417. Stated that the place where they hear the most noise is in their attic, which is above the height of the noise barrier. Inquired if a new noise barrier will reduce the noise heard at that height. 	<ul style="list-style-type: none"> Thank you for your email dated February 22nd, 2017 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). The replacement of the existing steel noise barriers from Island Park Drive to Lyon has been grouped with the Ministry's bridge replacement /rehabilitation project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor. The existing noise barrier will be reviewed as part of the detail design. A noise assessment will be carried out in accordance with MTO Environmental Guide for Noise to determine the height and type of noise barrier to mitigate 	WSP provided a response by email on March 21, 2017.

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				<p>outdoor noise levels. The MTO Environmental Guide for Noise requires the assessment of future noise levels in the outdoor living areas (typically rear yards) of Noise Sensitive Areas. MTO Noise Policy does not include the assessment of indoor noise levels.</p> <ul style="list-style-type: none"> • New noise barriers will be (durisol) concrete barrier. Concrete barriers are designed to absorb sound energy, and have been proven to be more effective than steel barriers which reflect noise. • Within the Queensway corridor clear sound barrier panels are being considered at structural (overpass) locations to provide vistas of the City, and reduce the weight of noise barriers on the structures. 	
22	February 28, 2017 / Website Comment	Public	<ul style="list-style-type: none"> • Inquired about the tentative schedule for the replacement of the noise barriers on the north side of Highway 417 • Noted that they are residents of the Wellington Village area and are looking forwards to the upgrade 	<ul style="list-style-type: none"> • Thank you for your e-mail February 28, 2017 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). • The replacement of the existing steel noise barriers from Island Park Drive to Lyon has been grouped with the Ministry's bridge replacement /rehabilitation project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor. • Construction is tentatively scheduled for 2019-2022, subject to regional priorities and environmental approvals. Given the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability. 	WSP provided a response by email on March 21, 2017.
23	March 1, 2017 / Email	Public	<ul style="list-style-type: none"> • Stated that their house faces an existing noise barrier along Highway 417. • Referenced the notice received by mail that only the noise barriers on the south side of Highway 417 would be replaced between Island Park and Preston Street. Wanted confirmation if the replacements to the north side are proceeding. • Stated that the noise barriers on the south side require replacement as they are continuing to rust, posing concerns for the health of residents and their children. 	<ul style="list-style-type: none"> • Thank you for your e-mail March 1, 2017 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). • This project entails the replacement of the existing steel noise barriers from Island Park Drive to Lyon and has been grouped with the Ministry's bridge replacement /rehabilitation project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor. • Construction is tentatively scheduled for 2019-2022, subject to regional priorities and environmental approvals. Given the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability. 	WSP provided a response by email on March 21, 2017.
24	July 9, 2016 and March 5, 2017 / Email	Public	<ul style="list-style-type: none"> • July 9, 2016: Inquired about the timing of the replacements of the noise barriers along the Island Park Drive westbound off-ramp. Noted that the noise barrier is in need of replacement. • March 5, 2017: Noted that they have been lobbying for the replacement of the noise barriers on the north side of Highway 417 from Parkdale Avenue to Island Park Drive and along the Island Park Drive off-ramp. Stated that they were pleased that the work will be completed • Requested that the new noise barrier at the Island Park off-ramp extend further down the off-ramp as there are residences that are in close proximity to Highway 417. Noted that the extension of the noise barrier would reduce noise, pollution, litter control and vehicle visibility. • Expressed concerns about the speed of traffic on the Island Park off-ramp as they exit Highway 417. Noted that the ramp is long and many vehicles do not slow down at the end. Inquired if it is possible to install a flat, low speed bump at the end of the ramp where the off-ramp connects to Island Park Drive. • Inquired about the proposed timing for the noise barrier replacements. 	<ul style="list-style-type: none"> • Thank you for your emails dated July 9, 2016 and March 5, 2017 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). • The existing noise barrier in question will be reviewed as part of the detail design that has recently commenced for the replacement/ rehabilitation of 10 bridges (5 sites) on Highway 417 between Preston Street and Percy Street, as well as operational improvements on Highway 417 from Island Park Drive to east of Lyon Street, in the City of Ottawa (GWP 4173-15-00). • Your suggestion to extend the existing noise barrier along the westbound off-ramp at Island Park Drive will be reviewed as part of a noise study assessment that will take into account the specific characteristics of the study area. • Modifications to the Island Park off-ramp are not part of this study. Speed bumps would have significant negative impacts on the safety and operations of provincial highway ramps and will not be considered. • Motorists may not be obeying the regulatory posted speed limit as they are approaching the municipal road. This becomes an issue of enforcement. MTO will advise the National Capital Commission and the Royal Canadian Mounted Police by copy of this response. • Construction is tentatively scheduled for 2019-2022, subject to regional priorities and environmental approvals. Given the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability. 	WSP provided a response by email on March 21, 2017.
25	March 21, 2017 / Website Comment	Public	<ul style="list-style-type: none"> • Inquired if the replacement of the noise barrier will impact the woods behind their property • Inquired if the new wall will be higher and more sound resistant than the current wall and if it will be more effective at protecting the air quality around the homes along Highway 417 • Stated that they would like to be added to the planning committee and help inform their neighbours 	<ul style="list-style-type: none"> • Thank you for your e-mail March 21, 2017 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). • This project entails the replacement of the existing steel noise barriers from Island Park Drive to Lyon and has been grouped with the Ministry's bridge replacement /rehabilitation project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor. • At this point in the study we are reviewing the proposed design with respect to any property and environmental impacts and will provide a more detailed response when the review has been completed and the design has advanced to a stage where we can determine exact impacts. 	WSP provided a response by email on March 23, 2017.

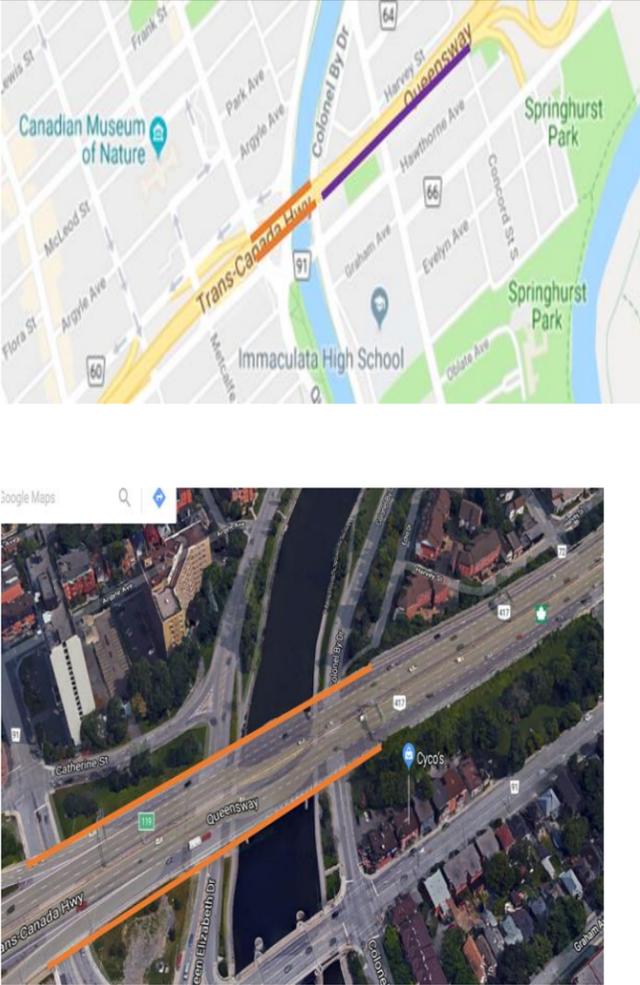
Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				<ul style="list-style-type: none"> The existing noise barrier will be reviewed as part of the detail design. A noise assessment will be carried out in accordance with MTO Environmental Guide for Noise to determine the height and type of noise barrier to mitigate outdoor noise levels. New noise barriers will be (durisol) concrete barrier. Concrete barriers are designed to absorb sound energy, and have been proven to be more effective than steel barriers which reflect noise. Previous studies indicated that no impact to air quality is anticipated. You have been added to our mailing list and will receive notices of the project's activities. This study does not have a public advisory committee. 	
26	April 4, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Enquired about the approximate start date for the replacement of the noise barriers along the north side of Highway 417 from Island Park Drive to east of Parkdale Avenue. 	<ul style="list-style-type: none"> Thank you for your e-mail April 30, 2017 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). The replacement of the existing steel noise barriers from Island Park Drive to Lyon Street has been grouped with the Ministry's bridge replacement /rehabilitation project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor. Construction is tentatively scheduled for 2019-2022, subject to regional priorities and environmental approvals. Given the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability. 	WSP provided a response by email on May 2, 2017.
27	June 8, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Requested to be added to the project mailing list. Noted that they were concerned about the impact the replacement of the noise barrier will have. 	<ul style="list-style-type: none"> Thank you for your e-mail dated June-08-17 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). We have added you to our mailing list. 	WSP provided a response by email on June 9, 2017.
28	June 8, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Requested to be added to the project mailing list. 	<ul style="list-style-type: none"> Thank you for your e-mail dated June-08-17 regarding the replacement of the existing noise barriers on Highway 417 (Ottawa Queensway). We have added you to our mailing list. 	WSP provided a response by email on June 9, 2017.
29	August 12, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Requested to be added to the project subscription list and to be notified of any future developments. 	<ul style="list-style-type: none"> Thank you for your e-mail dated August 24, 2017. We have added you to our mailing list. 	WSP provided a response by email on August 29, 2017.
30	August 14, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Inquired if a new noise barrier is to be installed at the former City yard that is to be incorporated into Reid Park (within the project limits). If so, they inquired if there is a tree replacement plan. 	<ul style="list-style-type: none"> Thank you for your e-mail August 24, 2017 regarding the existing noise barriers on Highway 417 (Ottawa Queensway). This project entails the replacement of the existing steel noise barriers from Island Park Drive to Lyon Street, and has been grouped with the Ministry's bridge replacement /rehabilitation project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor. As per your comment regarding the extension of the existing noise barrier to close the gap adjacent to Reid Park; the Ministry has reviewed this particular location in the past as the area in question presents a challenge with regards to noise barrier design and construction. The key issue is the presence of a large diameter City watermain in very close proximity to the edge of the rock outcropping (limit of the gap) that is in conflict and prohibits the installation of noise barrier at this location until such time that the City of Ottawa relocates the watermain. 	WSP provided a response by email on September 7, 2017.
31	October 25, 2017 / Website Comment	Public	<ul style="list-style-type: none"> Inquired about the timing of the construction of the Bronson Ave. interchange improvements, and the installation of noise barriers on the south side of the Queensway between Bronson and Lyon. Inquired if there were any possible remedial noise reduction measures. 	<ul style="list-style-type: none"> Thank you for your e-mail October 25, 2017 regarding the existing noise barriers on Highway 417 (Ottawa Queensway) at the Bronson Avenue interchange. The detail design and Environmental Assessment for the Highway 417 Bronson Avenue Interchange (WP 4089-17-01) has been grouped with the Ministry's bridge replacement /rehabilitation project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor. A noise assessment will be carried out in accordance with MTO Environmental Guide for Noise to determine the height and type of noise barrier to mitigate outdoor noise levels. Construction is tentatively scheduled for 2019-2022, subject to regional priorities and environmental approvals. Given 	WSP provided a response by email on October 25, 2017.

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				<p>the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability.</p> <ul style="list-style-type: none"> You may follow the detailed design at the following website: http://www.highway417-midtownbridgesandimprovements.com. 	
32	February 10, 2018 / Website Comment	Bike Ottawa	<ul style="list-style-type: none"> Requested update on the detour plan for the Highway 417 bridge replacement near Preston Street/O-train multi-use path. Requested to be added to the mailing list 	<p>Thank you for submitting a comment through the Highway 417 Bridge Replacements / Rehabilitations (Mid-town Bridges Project) website, on February 10, 2018.</p> <p>As requested, we have added you to the mailing list for the Mid-Town Bridges Project and you will be notified directly of all future consultation opportunities. We will be responding to your concerns regarding the Highway 417 CPR/O Train Bridges Project in a separate email.</p> <p>Should you require additional information, please feel free to contact me.</p>	WSP responded via email on March 14, 2018.
33	April 26, 2018 / Website comment form	Public	<ul style="list-style-type: none"> Requested status of Highway 417 Noise Barrier Retrofits Between the O-Train and the Rideau Canal project. 	<p>Thank you for your e-mail April 26, 2018 regarding the status of the noise barrier retrofits on Highway 417 (Ottawa Queensway).</p> <p>The noise barrier retrofits have been grouped with the Ministry's bridge replacement /rehabilitation project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor.</p> <p>Construction is tentatively scheduled for 2019-2022, subject to regional priorities and environmental approvals. Given the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability.</p> <p>Regards,</p>	WSP responded via email on May 5, 2018.
33a	May 15, 2018 / Email	Public	<ul style="list-style-type: none"> Noted they are aware of the bridge replacement projects, and that they are already on the distribution list for updates regarding those projects. Indicated they are most interested in the plans regarding replacement / retrofit of the noise barriers in the areas highlighted in orange in the two images below. The barriers in these locations are very low and provide negligible noise attenuation. As a result, this leads to very high levels of traffic noise in the neighbourhoods to the NW and SE of Highway 417, especially when the wind is blowing from the northerly or southerly directions. Inquired if there are any plans to improve the noise barriers in this area? If no, why not? Noted a similar situation exists further east, where there are no noise barriers along the bridge over the Rideau River. This results in high levels of traffic noise to areas north and south of the 417. Inquired if there are there any plans to provide noise attenuation in that area? Noted the other general area they area interested in is the location shown in purple in the map below. The noise barriers in this location are steel and starting to corrode and become unsightly. Inquired if there are any plans to replace these barriers with the new style barriers? 	<p>The ministry's noise barrier retrofit policy is based on addressing, where feasible, noise impacts to outdoor living areas of residential properties. MTO has undertaken a noise barrier retrofit study for Highway 417 in downtown Ottawa to determine the effectiveness of potential noise barriers. As a result of this study, a retrofit noise barrier is proposed on the south side of Hwy 417 from Kent St to Elgin St. Construction of this noise barrier is planned to occur in conjunction with the rehabilitation of the Bank St, O'Connor St, Metcalfe St and Elgin Street structures.</p> <p>No noise barriers are contemplated between Elgin Street and Colonel By Drive, as these would provide minimal attenuation to residential properties. The existing low barriers over the Rideau Canal Bridge, while similar in appearance to noise barriers, are to prevent snow from being plowed onto the Rideau Canal, Colonel By Drive and Queen Elizabeth Drive.</p> <p>Similarly, no noise barriers are proposed over the Rideau River, as there are no residential noise receivers that would benefit from such a noise barrier.</p> <p>The ministry has long term plans to replace the existing noise barriers on Highway 417, including those on the north and south sides of Hwy 417 from the Rideau Canal easterly to Nicholas Street. It is currently envisioned that these existing noise barriers would be replaced in conjunction with the rehabilitation (or replacement) of the Highway 417 bridge over Main Street. The ministry is currently undertaking an Environmental Assessment for rehabilitation or replacement of several downtown bridges, including Metcalfe Street, Elgin Street, Rideau Canal and Main Street. If you would like your name to be added to the mailing list for this EA study, please advise by return e-mail.</p>	MTO replied via email on May 14, 2018:

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
					
34	July 21, 2018 / Email	Public	<ul style="list-style-type: none"> Indicated they previously requested that MTO consider extending the noise barrier on the westbound side of the Island Park Offramp (exit 123) a little farther down the offramp, in which MTO indicated: The existing noise barrier in question will be reviewed as part of the detail design that has recently commenced for the replacement/ rehabilitation of 10 bridges (5 sites) on Highway 417 between Preston Street and Percy Street, as well as operational improvements on Highway 417 from Island Park Drive to east of Lyon Street, in the City of Ottawa (GWP 4173-15-00). Indicated they have concerns with safety as the adjoining slope to the westbound offramp is used by children to toboggan and sled in the winter. Noted some years there is a "snow fence" at the top of the slope along the offramp, but this offers no protection if a vehicle were to leave the offramp. Just extending the noise barrier a couple of sections would be much safer. Requested that MTO seriously consider this request to extend the noise barrier here. 	<p>Thank you for your email dated July 21st, 2018 regarding the westbound Island Park off ramp and safety concerns with respect to children using the areas adjacent to the ramp for tobogganing during the winter months.</p> <p>As noted in previous correspondence, the noise barriers along the Queensway westbound off ramp at Island Park Drive will be reviewed as part of a detail design that is ongoing for this area and will include a noise study to review the specific characteristics of the area. The question of whether an extension would be considered along the ramp will be addressed during the study to determine the effectiveness of potential noise barriers and the limits required should noise attenuation be warranted. The Ministry does not consider the installation of noise barriers as effective means of protection from traffic.</p> <p>In light of your concern the Ministry will assess and consider the need for appropriate mitigation in the context of safety which may include the installation of protection measures and or fencing within the limits described in your email in accordance with policy and guidelines for road side safety measures within the MTO right of way. Without an appreciation of the exact location(s) of concern; please note that the lands immediately adjacent to the westbound off ramp are owned by the National Capital Commission (NCC) and any concerns regarding activities and public safety should be forwarded and brought to the attention of NCC representatives.</p>	MTO replied via email on July 24, 2018.

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				I appreciate you bringing forward your concerns for consideration in the detail design. Should you have any questions or wish to discuss, please do not hesitate to call.	
35	August 17, 2018 / Email	Public	<ul style="list-style-type: none"> As a resident of the affected area they inquired whether the study is proceeding according to the published schedule and whether there is a timeline for implementing recommendations coming out of the study. Specifically, they area interested in whether there is a schedule for replacement of the sound barriers on the south side of Highway 417 from Parkdale Avenue to Preston Street. 	<p>Thank you for your e-mail August 17, 2018 regarding the status of the noise barrier replacements on Highway 417 (Ottawa Queensway).</p> <p>The noise barrier replacements have been grouped with the Ministry's bridge replacement project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor.</p> <p>Construction is tentatively scheduled for 2020-2023, subject to regional priorities and environmental approvals. Given the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability.</p>	WSP replied via email on August 20, 2018.
36	August 31, 2018 / Website Comment	Public	<ul style="list-style-type: none"> Requested the planned dates for the Bronson Avenue overpass replacement and realignment of the Bronson Avenue offramp. 	<p>Thank you for your email August 31, 2018. Construction is tentatively scheduled for 2020-2023, subject to regional priorities and environmental approvals. Given the scope and magnitude of the project, and the various elements of work to be completed; final construction schedules for the sequencing of bridge rehabilitations/rehabilitations, operational improvements and noise barrier replacements will be taken into account during the detail design, and implemented during construction with consideration for traffic operations, construction staging, operational constraints, and constructability.</p>	WSP replied via email on September 5, 2018.
37	September 10, 2018 / Website comment	Public	<ul style="list-style-type: none"> Inquired if there is an updated timeline for the project. Noted they are particularly interested in the noise study and barrier design. 	<p>Thank you for your email September 10, 2018.</p> <p>Given the scope and magnitude of the project, the various elements of work to be completed, and adjacent Highway 417 projects, submission schedules have been revised/deferred. A noise assessment is being under taken in accordance with MTO Environmental Guide for Noise during detail design to determine the height and type of noise barrier to mitigate outdoor noise levels. The availability of the environmental documentation is tentatively scheduled for Summer 2019. The website study schedule has been updated.</p>	WSP replied via email on September 14, 2018.
38	August 14, 2019 / Email	Public	<ul style="list-style-type: none"> I am writing to ask why the noise barriers at the eastbound Parkdale off ramp were never updated/replaced when the road was widened in 2017, which was part of the requirements (as noted on this site.) This, in addition, to the current widening of Highway 417 at Maitland, has meant a noticeable increase in the noise level for the neighbourhoods flanking the Queensway. It is particularly noticeable on the south side of the Queensway close to the eastbound Parkdale off ramp. Two summers ago, residents closest to the Highway received notification that the barriers would be replaced. We are still waiting for that to happen, and have had no updates on the timelines. 	<p>Thank you for submitting a comment via the Queensway Expansion East website regarding the replacement of the noise barriers located at the Highway 417 eastbound off-ramp at Parkdale Avenue.</p> <p>The noise barriers are currently planned to be replaced during the 2021 construction season as part of the Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00) (Highway 417 Midtown Bridges Project).</p> <p>Please note that the construction schedule is subject to regional priorities and environmental approvals. As well, the work sequencing may vary based on the Contractor's schedule of operations The Contractor will be required to notify adjacent properties a minimum of one week in advance of construction start with a more detailed construction schedule.</p> <p>An online Public Information Centre (PIC) is currently being held for the Highway 417 Midtown Bridges Project at www.highway417-midtownbridgesandimprovements.com. Further information regarding construction schedule and traffic impacts can be viewed online as part of the online PIC material.</p> <p>Should you require additional information, please feel free to contact me.</p>	WSP provided a response by emailed letter on February 24, 2020.
39	September 7, 2019 / Email	Public	<ul style="list-style-type: none"> Requested an update on the impact of the replacement of the noise barrier (south side of Highway 417 from Island Park Drive going west) 	<p>Thank you for your continued interest in the Highway 417 Bridge Replacements and Operational Improvements project (Highway 417 Midtown Bridges project). We would like to provide a project update and more information regarding the noise barrier replacements near your place of residence.</p> <p>The replacement of noise barriers adjacent to Ruskin Street is currently anticipated to occur in 2021. Vegetation adjacent to the existing noise barriers will be cleared. All work will take place from Highway 417. No construction staging is required on Ruskin Street.</p> <p>Please note that the construction schedule is subject to regional priorities and environmental approvals. As well, the work sequencing may vary based on the Contractor's schedule of operations The Contractor will be required to notify adjacent properties a minimum of one week in advance of construction start with a more detailed construction schedule.</p>	WSP provided a response by emailed letter on February 24, 2020.

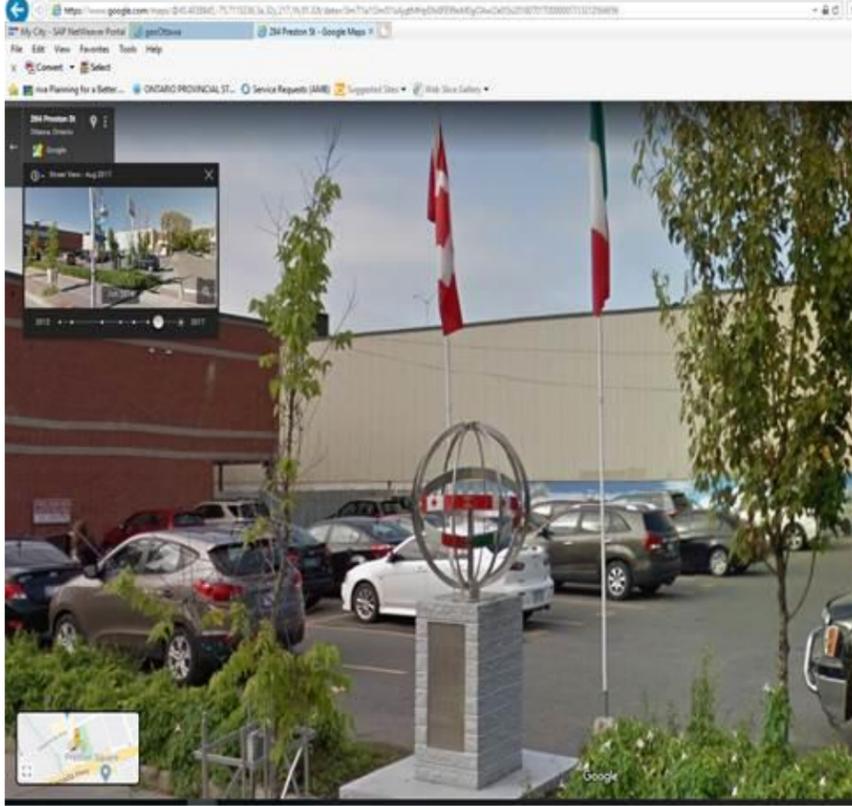
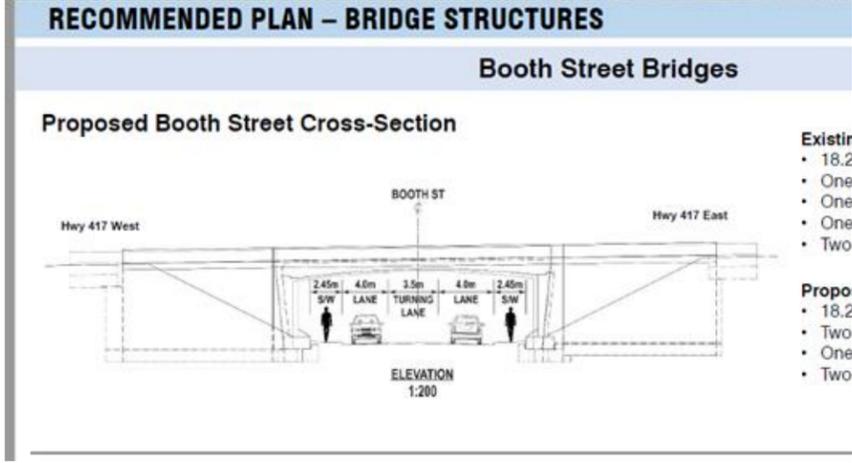
Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				<p>An online Public Information Centre (PIC) is currently being held for the Highway 417 Midtown Bridges Project at www.highway417-midtownbridgesandimprovements.com. Further information regarding construction schedule and traffic impacts can be viewed online as part of the online PIC material.</p> <p>Should you require additional information, please feel free to contact me.</p>	
40	November 15, 2019 / Bronson Avenue Website Comment Form	Public	<ul style="list-style-type: none"> Requested to be informed of on planned street closures and possible impact to our parking lot, if that information is currently available for the Natural Resources Canada parking lot. Requested to be added to project mailing list. 	<p>Thank you for submitting a comment through the Highway 417 Bronson Avenue Interchange Operational Improvements website.</p> <p>The Highway 417 Bronson Avenue Interchange Operational Improvement project is being coordinated with the Highway 417 Bridge Replacements and Operational Improvements Project (Highway 417 Midtown Bridges Project, GWP 4173-15-00) due to the overlap in the project limits. The Highway 417 Midtown Bridges Project includes the rapid replacement of the Preston Street, Rochester Street, Booth Street, Bronson Avenue and Percy Street bridges on Highway 417, as well as other operational improvements along the highway including noise barrier replacements.</p> <p>Closures will be required in areas surrounding the Natural Resources Canada (NRCAN) Booth Street Complex at Booth Street and Orangeville Street to facilitate construction operations for the Booth Street and Rochester Street bridge replacements and other associated construction works. Signed detour routes will be in place during all street closures. The following is a summary of the anticipated closures in this area:</p> <ul style="list-style-type: none"> Orangeville Street is anticipated to be closed from Rochester Street to Booth Street, and from Booth Street to just west of the NRCAN Complex parking lot entrance for the duration of construction (estimated 2021 to 2024). The entrance to the NRCAN parking lot shall be maintained at all times during regular business hours. Booth Street and Rochester Street will remain open for the majority of construction; however, each road will be fully closed at the bridges sites for a 3-week period to prepare for the rapid replacement of the bridges and to facilitate post-replacement construction operations. The Booth Street closure is anticipated to occur in July 2022 and the Rochester Street closure is anticipated to occur in August 2022. <p>Construction is tentatively scheduled for 2021-2025, subject to regional priorities and environmental approvals. As well, given the scope and magnitude of the project, the work sequencing may vary based on the Contractor's schedule of operations. The Contractor will be required to notify adjacent properties a minimum of one week in advance of construction start with a more detailed construction schedule.</p> <p>An online Public Information Centre (PIC) is currently being held for the Highway 417 Midtown Bridges Project at www.highway417-midtownbridgesandimprovements.com. Further information regarding construction schedule and traffic impacts can be viewed online as part of the online PIC material.</p> <p>Should you require any additional information, please feel free to contact me.</p>	WSP provided a response by emailed letter on February 24, 2020.
Public Information Centre (February 11, 2020) – Mail out date: February 5, 2020					
Indigenous Communities					
No comments received from Indigenous communities during PIC comment period					
Local Elected Representatives					
No comments received from Local Elected Representatives during PIC comment period.					
External Agencies					
41	February 7, 2020 / Email	City of Ottawa	<ul style="list-style-type: none"> Indicated they will review the PIC boards in terms of street lighting. 	--	No
42	February 18, 2020 / Email	Structural Appraisal Technologist, City of Ottawa	<ul style="list-style-type: none"> Provided the following questions / comments: Retaining Wall at Percy to be replaced; are there any anticipated impacts on City Municipal Yard at Catherine that is expected? Appears to be in close proximity. Noise barrier replacement in close proximity to Harmer Ave Ped Bridge and newly installed noise barriers installed under Ped Bridge replacement project. Please coordinate as/if necessary, construction activity for newly installed Harmer Ped Bridge still on-going. Upstand/retaining wall on Bronson adjacent to staging area for 417 Bronson and Percy Bridge replacement not in City inventory of retaining walls, ownership, 	<p>Thank you for your comments regarding the Highway 417 Midtown Bridges project. We would like to provide additional information on the project to address your questions.</p> <p>Retaining Wall at Percy Street The proposed work on the retaining wall located at Percy Street on the westbound Bronson Avenue off-ramp is primarily a rehabilitation of the existing wall. However, there is a need to complete some reconstruction to accommodate the new noise barrier to be installed on top of the existing retaining wall, as well as install the foundation for a new overhead sign structure. There is also a new retaining wall being constructed on the adjacent property to the east of the Municipal Yard. Given the retaining wall work to the east and the work required on / at the highway bridge structure at Percy Street, a portion of the Municipal Yard has been requested for use as a staging / work area to assist in these</p>	WSP provided a response by emailed letter on June 3, 2020.

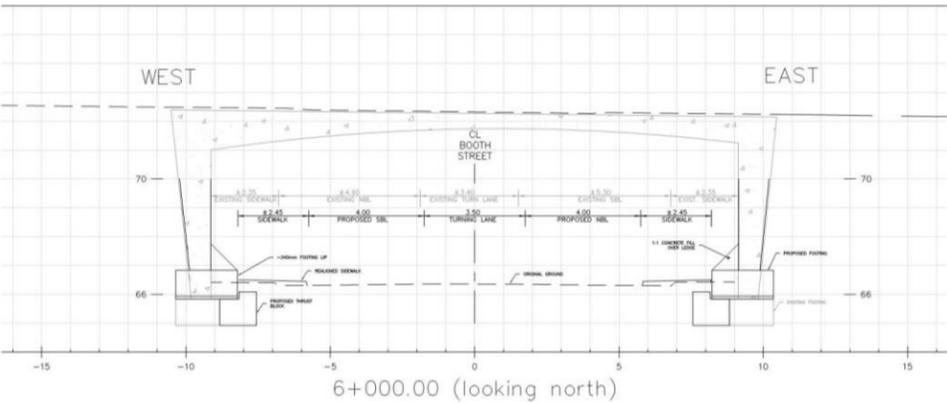
Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
			<p>maintenance not confirmed by City. Screen shot of wall below. Please advise of any expected impact.</p>  <ul style="list-style-type: none"> • Gateway feature SN019473 within road closure limits during rapid replacement for Preston close to Louisa St. This may require protection during construction activities, please advise of any anticipated impact, please protect as/if required. 	<p>construction operations. The Ministry of Transportation (MTO) has been in communication with the City regarding obtaining a Temporary Limited Interest during construction and is cognisant of the winter operations at this facility. The intent is to place restrictions within the Ministry's contract to ensure the construction occurs outside of the facility's winter operations schedule.</p> <p>Harmer Avenue Pedestrian Bridge All construction activities will be coordinated with the ongoing expansion of the Queensway between Maitland Avenue to Island Park Drive, as well as the new Harmer Avenue pedestrian bridge.</p> <p>Upstand / Retaining Wall on Bronson Avenue With the re-alignment of Chamberlain Avenue to opposite the existing E-N/S Bronson off-ramp, this small retaining wall will be removed, and the area regraded.</p> <p>Gateway Feature on Preston Street We can confirm that the gateway feature SN019473 will not be impacted during construction.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact me.</p>	

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
					
43	February 25, 2020 / Email	Project Manager, Cycling Program	<ul style="list-style-type: none"> Noted there is a discrepancy in the cross section for Booth Street at the Booth Street bridges replacement. There appears to be a narrowing of the cross section along Booth from the existing 18.28 m to 16.40 m based on adding up the street components as shown below. The note indicates a proposed span width of 18.28 m but the components don't add up to this total.  <ul style="list-style-type: none"> Noted concern given changing preferences for cycling infrastructure and increasing demand for bike lanes and cycle tracks on streets that formerly were 	<p>Thank you for your comments regarding the Highway 417 Midtown Bridges project. Please find responses to your comments regarding the Booth Street and Rochester Street cross-sections below.</p> <p>Booth Street Cross-section</p> <p>The existing clear span of the Booth Street bridges is approximately 18.28 m. This clear span length will be maintained as part of the proposed replacement bridges. However, the existing bridge footings are very shallow relative to the roadway and are too small to support the new bridges. The new bridge footings are larger and must be placed on top of the existing, resulting in the top of the footings well above the roadway (they cannot be buried like the other bridges). Providing a sidewalk at the same elevation as the top of footing (and maintaining the existing roadway width) would result in an approximate curb height of 650 mm above the adjacent roadway. This was deemed as an unacceptable condition.</p> <p>On the north side of Highway 417, at the intersection of Booth Street and Raymond Street, the sidewalk is required to be at the same elevation as the roadway to account for the crosswalks. These crosswalks are approximately 1.3 m north of the bridge on the east side of Booth Street, and 3 m on the west side of Booth Street. To transition within that short of a distance, stairs or a slope of severe gradient would be necessary. This was identified as an issue for both accessibility and maintenance.</p> <p>The existing cross-section from curb to curb has a width of 13.58 m with two 2.45 m sidewalks. The proposed width is 11.5 m, which includes two 4.0 m through lanes and a 3.5 m turn lane, with 2.45 m sidewalks. The width of the sidewalks can be reduced to 2.0 m which would then provide approximately 4.45 m through lanes, if necessary, to provide more road width.</p> <p>Please see below the cross-section for the Booth Street bridges.</p>	<p>WSP provided a response by emailed letter on June 3, 2020.</p>

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
			<p>considered acceptable with only shared use lanes. The existing span does allow for painted cycling lanes to be added or for full cycle tracks if the turning lane became unwarranted in the future. This would not be possible if the new cross section were narrowed to 16.4 m.</p> <ul style="list-style-type: none"> On a related note, the city is contemplating adding a designation to Rochester Street to make it into a part of the cycling network. This is in response to various other network issues that have arisen recently. No widening was requested for the Rochester underpass so we would absolutely understand if it is now impossible given the current project stage. Though it worthwhile posing the question, as years away from construction. 	 <p>Rochester Street Cross-section</p> <p>Widening of the Rochester Street structure to accommodate cycling lanes on Rochester Street would have severe impacts to traffic on Highway 417 and would require significant and long-term lane closures and disruption. The objective of the rapid bridge replacements and maintaining the same span length as existing, is to complete the work as quickly as possible with minimal disruption to the travelling public.</p> <p>We appreciate your comments and participation in the study. Should you have any further questions or comments, please do not hesitate to contact me.</p>	
Public					
44	February 11, 2020 / Website comment form	Public	<ul style="list-style-type: none"> Requested to be added to mailing list 	<ul style="list-style-type: none"> Contact added to mailing list for future correspondence 	No
45	February 11, 2020 / Website comment form	Public	<ul style="list-style-type: none"> Inquired why a noise barrier is not being built on the north side of highway 417 between Metcalfe St. and O'Connor St.? Noted the area adjacent to this is residential and a noise barrier would be beneficial. 	<p>Thank you for your interest in the Highway 417 Midtown Bridges project. We would like to provide more information to address your concerns regarding noise barriers on the north side of Highway 417 through downtown Ottawa.</p> <p>While the noise barriers in question are not part of this detail design study, the Ministry of Transportation (MTO) conducted a Highway 417 Centretown Noise Study in 2005 as part of the in the Preliminary Design and Environmental Assessment Study for Highway 417 (Ottawa Queensway) from Highway 416 Easterly to Anderson Road (GWP 663-93-00). The study evaluated areas along Highway 417 to determine where the construction of retrofit noise barriers could achieve a 5 decibel (dBA) reduction in noise levels at the first row of residences along the highway. The areas of investigation included from Bronson Avenue to the Rideau Canal on the north side of Highway 417.</p> <p>The study recommended that a noise barrier between Bronson Avenue and Lyon Street on the north side of the Queensway be added as a "Candidate Site" to MTO's list of recommended noise barrier retrofits. MTO maintains a list of recommended noise barrier retrofits that are reviewed periodically to identify priority sites for reconstruction. For the remaining areas along the north side of the Queensway reviewed as part of the 2005 study (between Lyon Street and the Canal), the study did not recommend noise barriers due to the high construction cost and poor noise barrier performance. For more information regarding plans for the downtown segment of the Queensway, including noise assessments, please refer to MTO's Ottawa Queensway Downtown Bridges Preliminary Design and Environmental Assessment Study (GWP 4170-13-00) project website: www.queenswaydowntownbridges.com.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact me.</p>	WSP provided a response by emailed letter on June 3, 2020.
46	February 11, 2020 /	Public	<ul style="list-style-type: none"> Inquired if the existing steel noise barriers located along the north side of Highway 417 from the new Harmer Avenue pedestrian overpass, west to Island Park 	See response to 45a.	

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
	Voicemail to Lincoln MacDonald (WSP)		Crescent will be replaced on the same convoluted path off of the highway, or if they will be replaced on the Highway 417 jersey barrier. Requested to be called back to discuss.		
46a	March 16, 2020 / Email to Lincoln MacDonald (WSP)	Public	<ol style="list-style-type: none"> 1. Is the new wall going to “match” the wall that was installed by Hughson last year in the immediate vicinity of the new overpass? 2. Will the new wall (west from the overpass on the north side) follow the current convoluted path (it jogs in and out on Helena and neighboring Geneva St) or will it be straightened out to follow the new Jersey barrier? Hopefully the plan is to run the new wall straight along the Queensway and plant trees etc in the area between the wall and the residences. 3. What are the timelines to replace this rusty wall? We understand the Harmer overpass is to be completed this year, hopefully the metal wall will shortly follow? 	<p>Good Afternoon Mr. Lockhart,</p> <p>I apologize for not getting back to you sooner. The project has had some changes due to external forces and I wanted to make sure I communicated the correct information to you.</p> <p>In responding I have followed your bullets.</p> <ol style="list-style-type: none"> 1.The new noise barrier wall is intended to match the noise barrier wall installed last year as part of the Harmer Pedestrian Bridge replacement undertaken by the City of Ottawa. The texture and colour scheme will be similar. 2.The new noise barrier wall will follow the highway / traffic barrier of the Queensway in a relatively straight line and the existing jog at Geneva and Helena will not be preserved. The existing noise barrier will be removed and these areas will not be fenced and the existing trees / shrubs will be allowed to flourish. There are no plans to do further landscaping of these areas and maintenance of these areas will be minimal as they will be allowed to naturalize. 3.The replacement of the existing noise barrier is subject to the construction schedule prepared by the Contractor; however, it is anticipated given the other planned works (bridge replacements, retaining walls, etc) included in the contracted works that the noise barrier will be replaced near the end of the construction period (projected to be 5 years). We currently project 2025 as the year for the noise barrier replacement at this location. Please keep in mind that the construction is subject to change pending final approvals and Ministry of Transportation Ontario funding priorities. <p>I trust the above addresses you inquiries. If you wish further information or clarification please feel free to contact me.</p>	WSP provided a response by email on March 16, 2020.
47	February 11, 2020 / Voicemail to Lincoln MacDonald (WSP)	Public	<ul style="list-style-type: none"> • Requested to be informed of the construction schedule for the replacement of noise barriers on south side of Highway 417 near Reid Avenue and Young Street. Indicated that the existing noise barriers are in need of replacement as noise is unbearable all year round and debris / salt from highway is impacting windows and houses. Requested the replacement to occur as soon as possible. 	<p>Thank you for your interest in the Highway 417 Midtown Bridges project. We apologize for the delay in responding to your voicemail. The project schedule has been under review due to coordination with other ongoing construction on the Queensway and the Global COVID-19 pandemic and we wanted to make sure we communicated the correct information to you.</p> <ul style="list-style-type: none"> • The replacement of the existing noise barrier is subject to the construction schedule prepared by the Contractor; however, it is anticipated given the other planned works (bridge replacements, retaining walls, etc) included in the contracted works that the noise barrier will be replaced near the end of the construction period (projected to be 5 years). We currently anticipate that the noise barrier replacement at this location will occur in the 4th year of construction. Please keep in mind that the construction is subject to change pending final approvals and Ministry of Transportation Ontario funding priorities. • A representative of MTO will to notify adjacent properties a minimum of one month in advance of construction start with a more detailed construction schedule. 	WSP provided a response by phone on June 3, 2020.
48	February 12, 2020 / Website comment form	Public	<ul style="list-style-type: none"> • Requested to be added to project mailing list. 	<ul style="list-style-type: none"> • Contact added to mailing list for future correspondence 	No
49	February 12, 2020 / Email	Public	<ul style="list-style-type: none"> • Noted Chief Pingitore retired in 2018 and has been replaced by Fire Chief Kim Ayotte. • Requested to update distribution list. 	<ul style="list-style-type: none"> • Contact information updated for future correspondence 	No
50	February 12, 2020 / Voicemail to Lincoln MacDonald (WSP)	Public	<ul style="list-style-type: none"> • Indicated they previously spoke to Lincoln MacDonald (WSP Project Manager) regarding noise barriers being erected from Bank Street to Elgin Street and beyond. • Requested to be called to discuss the project. 	<ul style="list-style-type: none"> • Thank you for your interest in the Highway 417 Midtown Bridges project. • Noise walls to be replaced as part of this project are between Island Park Drive and Kent Street. Noise walls east of Kent Street will be undertaken during future MTO construction projects. It is not possible to provide a timeline as this work is subject to change pending final approvals and Ministry of Transportation Ontario funding priorities. 	WSP provided a response by phone on June 3, 2020.

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
51	February 13, 2020 / Website comment form	Public	<ul style="list-style-type: none"> Inquired about noise barriers on Highway 417 south side between Bank Street and Nicholas Street off ramp. Are there any plans to install barriers there (for e.g. just south of the Museum of Nature) Requested to be added to project mailing list. 	<p>Thank you for your comments in response to the Highway 417 Midtown Bridges online Public Information Centre (PIC). We apologize for the delay in responding to your question; the project schedule has been under review due to coordination with other ongoing construction on the Queensway and the global COVID-19 pandemic.</p> <p>While not included as part of this detail design study, a new series of noise barriers on the south side of the Queensway from between Glendale Avenue and the Rideau Canal was recommended as part of a separate study carried out by the Ministry of Transportation (MTO) in 2005. As well, the Preliminary Design and Environmental Assessment Study for Highway 417 (Ottawa Queensway) from Highway 416 Easterly to Anderson Road (GWP 663-93-00) recommended that the existing noise barrier be replaced and upgraded as construction occurs along the Queensway. Noise walls east of Kent Street will be undertaken during future MTO construction projects. It is not possible to provide a timeline as this work is subject to change pending final approvals and MTO funding priorities.</p> <p>A noise assessment of Highway 417 between the Rideau Canal and Nicholas Street is being completed as part of a separate Ottawa Queensway Downtown Bridges Preliminary Design and Environmental Assessment Study (GWP 4170-13-00). More information about the noise assessment and the project in general is available on the project website: www.queenswaydowntownbridges.com.</p> <p>We appreciate your interest in this study and have added you to the mailing list. Should you have any further questions or comments, please do not hesitate to contact me.</p>	WSP provided a response by emailed letter on June 3, 2020.
52	February 13, 2020 / Website comment form	Public	<ul style="list-style-type: none"> Inquired what year the project expecting to be completing the work between Bank Street and the canal including the erection of noise barriers on the south side of Highway 417. 	<p>Thank you for your comments in response to the Highway 417 Midtown Bridges online Public Information Centre (PIC). We apologize for the delay in responding to your question; the project schedule has been under review due to coordination with other ongoing construction on the Queensway and the global COVID-19 pandemic.</p> <p>While not included as part of this detail design study, a new series of noise barriers on the south side of the Queensway from between Glendale Avenue and the Rideau Canal was recommended as part of a separate study carried out by the Ministry of Transportation (MTO) in 2005. As well, the Preliminary Design and Environmental Assessment Study for Highway 417 (Ottawa Queensway) from Highway 416 Easterly to Anderson Road (GWP 663-93-00) recommended that the existing noise barrier be replaced and upgraded as construction occurs along the Queensway. Noise walls east of Kent Street will be undertaken during future MTO construction projects. It is not possible to provide a timeline as this work is subject to change pending final approvals and MTO funding priorities.</p> <p>No work will be completed between Bank Street and the Canal as part of this project; however, MTO is currently undertaking a separate Ottawa Queensway Downtown Bridges Preliminary Design and Environmental Assessment Study (GWP 4170-13-00) within that segment of the Queensway. More information about that project is available on the project website: www.queenswaydowntownbridges.com.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact me.</p>	WSP provided a response by emailed letter on June 3, 2020.
52a	June 4, 2020 / Email	Public	<p><i>Comments on June 3, 2020 response:</i></p> <p>Noted they received a message 6 years ago indicating that noise barriers would be added to the south side of the Queensway from Bronson Avenue and the Rideau Canal. Requested an explanation of what has changed to take that off the table as the response appears to indicate.</p>	<p>I would like to clarify the response we sent to you by email last week regarding noise walls on the south side of Highway 417 between Bank Street and the Canal.</p> <p>WSP has been retained to complete the Detail Design for Highway 417 Bridge Replacements and Operational Improvements (the "Midtown Bridges" project), which includes improvements to segments of noise barrier and retaining walls on Highway 417 from approximately Island Park Drive to Lyon Street.</p> <p>The intent of our letter sent to you last week was to confirm that noise walls on the south side of Highway 417 from Bank Street to the Canal will not be constructed as part of the Midtown Bridges project. The Ministry of Transportation still intends to build noise walls between Bank Street and the Canal as was confirmed to you previously, but this will be done in coordination with other future construction projects planned for that section of the Queensway.</p> <p>As part of the Midtown Bridges (this project), the existing metal noise barrier on the south side of the highway from Bronson Avenue to Kent Street will be replaced. Overall construction for the Midtown Bridges project is anticipated to start in early 2021, however the construction start date is subject to change based on the timing of the contract award, environmental approvals and Ministry priorities. The timing of the replacement of the noise barrier is also subject to the construction schedule to be determined by the Contractor.</p> <p>As part of the previous work between Kent Street and Bank Street, new noise walls have already been installed by the</p>	WSP provided a response by email June 8, 2020

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				<p>Ministry.</p> <p>For additional information regarding the Ottawa Queensway downtown bridge and noise barrier construction (between Bank Street and the Rideau Canal), please refer to the project website: www.queenswaydowntownbridges.com.</p> <p>I apologize for any confusion caused by the wording in the letter we sent to you last week. If you have any further questions, please feel free to contact the project team copied here.</p>	
53	February 16, 2020 / Website comment form	Public	<ul style="list-style-type: none"> • Re: 458 Catherine Street • Nice to see it demolished. The new earth bank will rise to the level of the Queensway if I read this document correctly and slope down to Catherine Street. • This is going to eliminate private, paid parking on that lot. Could you not structure this project to retain some private parking on the lot similar to what exists on Catherine immediately west of Bronson. As in all downtown areas, parking is at a premium there and this would be a source of income for the City (perhaps it is the Province) at that point. 	<p>Thank you for your comments regarding the Highway 417 Midtown Bridges project.</p> <p>Following construction, the 458 Catherine Street property will be the site of a “dry pond,” which will be used to retain excess stormwater / runoff from the Highway during heavy rainfall events. Most of the time, the pond will remain dry and contain no water. Low-maintenance landscaping will be planted on the site and will include plants that are appropriate for this kind of stormwater management feature. The site will be fenced, as required by Ministry policy for Highway Corridors.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact me.</p>	WSP provided a response by emailed letter on June 3, 2020.
54	February 18, 2020 / Website comment form	Public	<ol style="list-style-type: none"> 1. What will happen to the beautiful wall art under the Preston Street bridge? I really hope it will be restored after the bridge replacement although I see nothing about it in the slide deck. 2. I assume that the Trillium pathway will be reopened under the 417 prior to Preston being closed. Otherwise, there will be no easy detour for pedestrians and Preston is a busy pedestrian street. 3. Hopefully there will be no impact on Italian Week Festival activities with this... 4. I'm looking forward to the bridge being replaced, rather than repaired frequently as has been happening. 	<p>Thank you for your comments regarding the Highway 417 Midtown Bridges project. Over the course of this project, we have been coordinating with the Preston Street Business Improvement Association (BIA) to address impacts to the mural under the Preston Street Bridge, streetscaping features, the Italian Week Festival, and other concerns. We would like to provide additional information on the project to address your questions and concerns.</p> <p>Preston Street Bridge Mural Reinstatement</p> <p>The mural underneath the Preston Street bridge will be removed prior to construction and reinstated by the Preston Street BIA upon completion of the bridge replacement.</p> <p>Trillium Multi-use Pathway Detour</p> <p>The Trillium Multi-Use Pathway detour that is currently in place is City of Ottawa infrastructure. This detour was constructed for the rapid replacement of the Highway 417 bridges over the O-Train corridor that is currently under construction. The detour will remain for the duration of the construction of the Stage 2 Light Rail Transit (LRT) Trillium Line extension. The existing Trillium Multi-Use Pathway will be reinstated and reopened by the City once Stage 2 LRT work in that area is complete. The pathway will be reinstated in advance of the planned closure of Preston Street to pedestrians for the Preston Street bridge rapid replacement.</p> <p>Impacts to Italian Week Festival</p> <p>The Contractor will not be permitted to start work impacting Preston Street until after the Italian Week Festival.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact me.</p>	WSP provided a response by emailed letter on June 3, 2020.
55	February 19, 2020 / Mailed letter	Public	<ul style="list-style-type: none"> • Interested in the noise barriers; have lived at their address for over 40 years and was here before the last barrier was put in place. Needless to say, it was completely useless! I was told the last time that they couldn't build the barrier high on our side of the Queensway (we are higher than the other side) because the noise would reflect over to the other side. Well too bad. I believe we should all get some relief from the constant sirens (police, fire and ambulance), air brakes, motor bikes, etc. I am hoping that this new barrier will give a bit of sound reduction. MOT tends to take care of traffic. Perhaps a little concern could be given to those of us backing on the 417. Hoping to hear that this time the job gets done right. 	<p>Thank you for your comments received regarding the Highway 417 Midtown Bridges project.</p> <p>The noise wall at the rear of your property will be replaced with a new 5 m high noise wall. To ensure consistency along the Highway 417 corridor, the MTO has adopted a standard 5 m height for all new noise wall installations. This is approximately 1 m higher than the existing noise wall. As well, the new noise wall will be absorptive versus the old noise wall which is reflective.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact me.</p>	provided a response by mailed letter on June 8, 2020.
56/56a	February 20, 2020 / Website comment	Public	<ul style="list-style-type: none"> • Indicated they reviewed the documents found on https://www.highway417-midtownbridgesandimprovements.com/ thanks to our city councillor, Jeff Leiper, linking to them in a recent ward newsletter. I live on _____ west of _____ where the noise barriers are to be replaced. The questions I would like answered 	<p>Thank you for your comments regarding the Highway 417 Midtown Bridges project. We apologize for the delay in responding to your questions. We would like to provide additional information on the project to address your concerns.</p> <p>Highway 417 Westbound Island Park Drive Off-ramp Closure</p>	WSP provided a response by emailed

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
	form and email to Lincoln MacDonald		<p>are the following:</p> <ul style="list-style-type: none"> • 1) the Island Park exit off ramp shows in year 1 to be closed for 5.5 months - what is the reason for the closure - is it for the noise barrier replacement, or adding of noise barrier at the offramp, or moving the offramp to add the additional lane to the 417, or for all of these? • 2) is year 1, intended to be 2020? I.e. will the offramp be closed this calendar year through the summer and fall months as indicated? If not 2020, what year is going to be "year 1", and what year will the noise barriers actually be replaced at Island Park Cres? It was already work that was in plans earlier for the year 2016, I believe, so the residents nearby have been wondering for quite some time when all the holes in the noise barriers would be fixed by replacement • 3) the plan indicates the work area around the Island Park Drive offramp includes some of the space right next to the offramp in the park - will any park space be lost? Many, many neighbourhood children use the slope daily to toboggan in the winter next to the offramp - it seems it would be a safe idea to extend the noise barriers during replacement, along the side of the offramp, by about 50 meters to include this area • 4) what calendar year will the noise barriers on the north side of 417, east of IPD, be replaced? • 5) during replacement, how long will there be no noise barriers on Island Park Cres, resulting in significantly increased noise transmission? • 6) on slide 24 of the PDF on this site "GWP 4173-15-00_FINAL_Online PIC_Feb 11.pdf", the offramp to IPD is marked in yellow for a portion of it - what does this indicate, and what construction will take place in that yellow section next to the offramp? • 7) if the roadway on Island Park Cres is damaged by construction equipment on it for months long periods, I expect it will be repaired or repaved - is this correct? • 8) why do we not receive any notification of the review by mail, or in our mailboxes, to those nearby that will be affected by the work? If it was not for my city councillor, I would have not found the documents on this site • 9) will any removed vegetation or trees at the work area in the park space next to the offramp be replaced, or saved, to protect the already reduced number of trees in the park 	<p>The Island Park Drive westbound off-ramp closure is required to complete noise barrier replacements, retaining wall rehabilitation and localized storm sewer replacement.</p> <p>Anticipated Construction Schedule</p> <p>The proposed project schedule is currently being reviewed and coordinated with other ongoing projects on Highway 417. Overall construction is anticipated to start in early 2021, however the construction start date is subject to change based on the timing of the contract award, environmental approvals and Ministry priorities. The timing of the replacement of the noise barrier is also subject to the construction schedule to be determined by the Contractor.</p> <p>A representative of MTO will be required to notify adjacent properties a minimum of one month in advance of construction start with a more detailed construction schedule.</p> <p>Island Park Drive Construction Staging Area and Noise Barrier Location</p> <p>The noise barriers along the Island Park Drive off-ramp will be replaced in the same location as existing. The construction staging area near Island Park Drive, as shown on Slide 24 of the online Public Information Centre (PIC) display boards, will be used to complete the noise barrier replacements and retaining wall rehabilitation. Tree removals and excavation are required to access the top of the footings of the existing retaining wall and complete the rehabilitation works. The construction staging area will be fenced to ensure safety of the public during construction. Impacts to these areas will be temporary; there will be no permanent loss of park land / open space.</p> <p>Vegetation Removals</p> <p>Vegetation removals will be kept to the minimum required to complete the work. A landscape plan is currently being developed to reinstate areas disturbed by construction. Replacement tree species will be selected to provide visual interest through a mix of deciduous, coniferous and indigenous species, with an emphasis on the use of urban tolerant native plants as appropriate, and in keeping with the Context Sensitive Design (CSD) guidelines implemented elsewhere along the Highway 417 corridor.</p> <p>During replacement, how long will there be no noise barriers on Island Park Cres, resulting in significantly increased noise transmission?</p> <p>Given the nature of the work on the retaining wall, it is anticipated that noise barrier will not be in place for approximately 3-6 months. The contract will have restrictions in place that require the contractor to replace the noise wall as quickly as possible.</p> <p>Damage to Island Park Crescent during Construction</p> <p>The Contractor will be required to restore Island Park Crescent to pre-existing condition upon completion of construction. If damage to the roadway does occur during construction, the roadway will be repaired.</p> <p>Online Public Information Centre Notification</p> <p>We apologize if you did not receive a direct letter notification of the online PIC. We make every effort to notify individuals who may be impacted or interested in the works of consultation opportunities through a combination of newspaper notices, direct letter / email notification, and the assistance of local Councillors and municipalities to share information with constituents. We have confirmed your email address (david@vangeyn.net) is on our project mailing list; you will receive email notification of future project updates. If you wish to receive updates by mail, please provide your mailing address so we may add this to our mailing list.</p> <p>Upon completion of the Detail Design for this project, a Design and Construction Report (DCR) will be prepared and filed for a 30-day public review period. The DCR will document the Class EA process followed, including a description of the Recommended Plan, potential environmental effects, and the final mitigation plan. You will be notified directly when the DCR is filed for the 30-day public review period.</p>	letter on June 3, 2020.

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact me.	
57	February 20, 2020 / Website comment form	Public	<ul style="list-style-type: none"> I am concerned for the birds & wildlife that have their homes in the green spaces along the Queensway. What will happen to them as you clear these areas. I live between [redacted] on the north side of the Queensway. The sound barriers there look fine. I see bunnies all year round & numerous birds in this fenced in, safe for critters zone. Some animals & birds are only seen in the winter. Then spring comes & there are also raccoons & more birds. Even hawks come by for visits. In the past few years, catbirds have been returning every year as well as many other bird species. Please consider these creatures. If the barrier isn't broke, don't fix it. Leave the green space alone please. 	<p>Thank you for your comments regarding the Highway 417 Midtown Bridges project. We apologize for the delay in responding. We would like to provide additional information on the project to address your concerns regarding wildlife and green space.</p> <p>The existing noise barriers along Highway 417 at this location have reached the end of their design service lifespan and will be replaced in support of identified highway improvement needs in the area, including; extension of the Bronson Avenue westbound onramp and replacement of the existing deteriorated retaining wall along Raymond Street between Booth Street and Lebreton Street North.</p>	WSP provided a response by emailed letter on June 3, 2020.
57a	May 27, 2020 / Website comment form and email to Lincoln MacDonald	Public	<ul style="list-style-type: none"> Hello, I contacted you before & I never heard back. I'm concerned about the replacement of the sound barrier between the Bronson on ramp & Booth St. It appears to be solid & I believe that it is a waste of time & money to replace it. Also, that area is a green space that is home to many birds & small animals. Currently they are all nesting & creating new life. There are year round animals & birds, & seasonal birds that call that area home. Please do not destroy their habitat. 	As part of the Environmental Assessment process followed for this project, we have completed natural environmental field investigations to document natural features that may be impacted by the works and identify specific mitigation measures to minimize impacts during construction. The results of the field investigations and assessment of impacts will be available for public review as part of the Design and Construction Report (DCR) that will be prepared near the completion of the project to document the Environmental Assessment process. We will notify you directly by email when the DCR is available for public review on the project website.	
57b	June 2, 2020 / Website comment form	Public	<ul style="list-style-type: none"> I write to you again with the concern of replacing the sound barrier between the Bronson on ramp (Bronson & Raymond) & Booth st in Ottawa. The barrier seems to be in good shape. Why are you replacing it? That area, by the sound barrier, is a fenced in natural urban green space that is home to small animals & birds. What are you doing to protect the wildlife there? Are you planning on cutting down any trees, grass or shrubs? If so, why & by how much? A natural urban green space like this is rare & spectacular. It is not only a habitat for wildlife, it absorbs air pollutants & noise, & is a positive mental health asset. Please respond to my concern. 	<p>The Contractor will be required to employ mitigation measures to minimize impacts to vegetation and wildlife during construction. Vegetation removals will be limited to those areas where it is required to complete the work. Wildlife incidentally encountered during construction will not be harmed and will be allowed to move away from the construction area on its own. Vegetation clearing and grubbing will be scheduled to occur outside of the breeding bird period (April 5 to August 31) to avoid harming breeding birds in accordance with the Migratory Birds Convention Act (MBCA). If vegetation clearing or grubbing cannot be avoided during the period when most birds in the area breed, it will be preceded by a bird's nest survey to ensure there are no active nests (nests with eggs or young birds) present. If an active nest is encountered, it will be protected.</p> <p>Following construction, areas that have been disturbed will be reinstated in accordance with a landscape plan that is currently under development. Replacement tree species will be selected to provide visual interest through a mix of deciduous, coniferous and indigenous species, with an emphasis on the use of urban tolerant native plants as appropriate, and in keeping with the Context Sensitive Design (CSD) guidelines implemented elsewhere along the Highway 417 (Ottawa Queensway) corridor.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact me.</p>	
57c	June 3, 2020	Public	<ul style="list-style-type: none"> I still have questions & concerns about the urban green space between the Bronson on ramp, at Raymond St, & Booth St, Ottawa. The letter I received mid February from WSP, says the noise barrier is being replaced from the Rochester westbound on ramp, east to Bronson Av. You say along Raymond from LeBreton St N to Booth St. The current noise barrier/retaining wall is cement from the Bronson on ramp, at Raymond, to almost Booth St. Just before Booth St, past LeBreton St N, the wall is metal & goes to the Rochester westbound on ramp. Are you replacing the cement noise barrier or just the metal one? The words that you will make attempts to minimize impacts to vegetation & wildlife during this construction is good. However, then there are words of vegetation removals, clearing & grubbing, & landscape plans. Are you planning on making this a manicured lawn? You also said about planting things of visual interest. To whom are these visual interests aimed at? This little green space has parts that are thickets. Will you leave that as is? The wildlife make their homes in that or under those thickets & it is also protection for the wildlife when the hawks come by. As is, if they get eaten by a hawk, & some of them do, it is a natural circle of life. Without the thickets, the wildlife lose that natural protection & the hawks will eat more than they should. Removing the 	<p>Thank you for your follow up questions. We hope the responses below help clarify our June 3, 2020 letter and address your concerns.</p> <p>We would also like to apologize for the delay in responding to your initial inquiry in February. While we attempt to respond to all comments received through our project website in a timely manner, we received a number of comments during the online Public Information Centre (PIC) period that required review of the project schedule in coordination with other ongoing construction activities and were delayed in issuing responses.</p> <p>Noise Barriers</p> <p>To further clarify the scope of the noise barrier work, we have attached the plan from our online PIC material that illustrates the location of noise barrier replacement throughout the project limits. Both the metal and concrete noise barriers on the north side of the highway located from approximately Preston Street to Bronson Avenue will be replaced. The green space you have identified will also be impacted by replacement of the existing retaining walls in that location as well as the extension of the Highway 417 westbound on-ramp at Bronson Avenue. Unfortunately these works cannot be completed without disturbance to this green space. As noted in the June 3, 2020 letter, vegetation removal will be minimized to the extent required for the Contractor to complete the work. Removal of vegetation is expected to be concentrated primarily between Booth Street and Bell Street.</p>	WSP provided a response by email on June 10, 2020

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
			<p>thickets harms the wildlife.</p> <ul style="list-style-type: none"> You mentioned a context sensitive design (CSD) & guidelines; Is this something your company made? Usually CSD involves talking to residents & understanding what makes a place unique. You are talking to me now & I appreciate it, however, I was not contacted after I made comments & asked to be replied to in February. Why not? I have lived beside this lovely little urban green space for 17 years & appreciated every minute of its natural beauty. Please leave it, as much as possible, to be the undisturbed piece of nature in the city that it is. 	<p>Landscape Plan and Context Sensitive Design Elements</p> <p>After construction, the area will be replanted with a mix of low maintenance native plant species; sod will be used in limited locations only. At intersection locations (e.g., Booth Street and Raymond Street) the corners will be enhanced by the use of perennials, ornamental shrubs and ornamental fencing to replace the existing chain link fence. Following construction the intent is to allow the slope areas to re-naturalize and re-establish ecological value. While the landscape plan for the area is still being finalized, this will be included as part of the Design and Construction Report (DCR) for this project.</p> <p>The Ministry of Transportation (MTO) developed Context Sensitive Design (CSD) guidelines for the Queensway corridor in 2011. The guidelines were developed in consultation with the National Capital Commission (NCC) and the City of Ottawa, and are intended to provide an overall design vision for the Queensway that recognize the highway's status as the entryway to Canada's Capital. The CSD guidelines include a suite of design elements for bridges, noise walls, retaining walls, landscaping, and other elements to be implemented along the highway as part of future projects. Some of these features can be seen along Highway 417 from Nicholas Street to Ottawa Road 174, and are being implemented as part of the current construction from Maitland Avenue to Island Park Drive.</p> <p>The CSD guidelines include recommendations for landscaping along the highway, identifying specific native plant species tolerant of a highway environment that should be incorporated where appropriate. The term "visual interest" is used in this context to refer to the selection of native plant species of various sizes, shapes, colors, and textures that have aesthetic appeal in all four seasons and also support urban wildlife.</p> <p>To build on the 2011 CSD guidelines, MTO also reviewed opportunities in 2016 to implement CSD features as part of the Preliminary Design for the Highway 417 Midtown Bridges project. This included development of a landscaping strategy for the study area to enhance the pedestrian space through the use of low maintenance, native plantings and includes naturalized planting to be set back on slopes to increase vegetative cover. The type of vegetation to be used includes species that are native, hardy, and tolerant of the urban roadside conditions.</p> <p>We appreciate your continued interest in this project. Should you have any further questions or concerns, please do not hesitate to contact the Project Team.</p>	
58	February 20, 2020 / Website comment form	Public	<ul style="list-style-type: none"> As a business owner located between _____ this has a great impact on my ability to run my business (and already has with the loss of street parking and with the addition of exceptionally dangerous bike lanes that shoots across parking exit). What is the project time frame (month/year) of the Preston St replacement? Does a "full street closure" mean I will be unable to carry on customary business during this time frame? ie: will the sidewalks be open? 	<p>Thank you for your comments regarding the Highway 417 Midtown Bridges project. Over the course of this project, we have been working with the Preston Street Business Improvement Area (BIA) to address concerns such as yours related to construction on Preston Street.</p> <p>Preston Street will be fully closed from Young Street to St. Anthony's Street during the preparatory works and rapid replacement of the Preston Street bridges. The duration of the full closure of Preston Street is three weeks and it is expected to occur in the summer of the 4th year of construction. It will be scheduled to avoid the Italian Week Festival in June. The sidewalks on Preston Street between St. Anthony Street and Louisa Street will remain open during the full closure and access to businesses will be maintained. Residents and businesses will be notified in advance of the closure of Preston Street to traffic.</p> <p>Street parking will be temporarily removed on St. Anthony Street to provide access to the Highway 417 retaining walls for repairs and on Preston Street between Louisa Street to Young Street during preparation for the rapid bridge replacements. The City of Ottawa parking lot north of Highway 417 across from St. Anthony Street will be temporarily closed during construction starting in summer of the 4th year of construction; however, a temporary parking lot will be constructed on the Adult High School Soccer field for public use during that time to compensate for lost parking spaces.</p> <p>Should you have any further questions or comments, please do not hesitate to contact me.</p>	WSP provided a response by emailed letter on June 3, 2020.
59	February 25, 2020 / Website comment form	Public	<ul style="list-style-type: none"> 1) General: timelines mentioned in the study are not specified. What is meant by "Year 1"? 2) From page 23: Noise barriers replacement. Area number 4 indicates its eastern boundary at Champagne. The current barrier runs all the way to Preston. Furthermore, west of Loretta the barrier is taller, and drops lower between Loretta 	<p>Thank you for your comments regarding the Highway 417 Midtown Bridges project. We would like to provide additional information on the project to address your concerns.</p> <p>Construction Timeline</p> <p>The proposed project schedule is currently being reviewed and coordinated with other ongoing projects on Highway 417. Overall construction is anticipated to start in early 2021, subject to environmental approvals and Ministry priorities.</p>	WSP provided a response by emailed letter on June 3, 2020.

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
			<p>and Champagne. This was likely because at the time of initial construction, there were no residences east of Loretta. Now that there are residences, the barrier needs to be higher at least past Champagne (between Loretta and east of Champagne). The noise especially at the section is quite high. As well in the winter ice chunks get thrown from the 417 into our parking area, causing safety concerns. What is the actual plan for that section?</p> <ul style="list-style-type: none"> • 3) From page 24: Areas impacted by construction and page 25: Construction staging areas. The yellow area on p24 covers our complex (yellow lines up Loretta and across the). Is this an error? A construction area has been established just east of our complex, between Champagne and O-train line. That area does not appear on page 24 or 25. Could you please confirm? • 4) As well, there is no indication as to what is going to be done to that area between Champagne and O-train line, once the construction work is done. Would it be possible to provide the details, as well as the timeline (year)? 	<p>Please note that the construction schedule is subject to the timing of the award of the construction contract. Furthermore, the work sequencing may vary based on the Contractor's schedule of operations.</p> <p>Noise Barrier and Snow Guard Replacement</p> <p>The section of existing barrier from Champagne Avenue to Preston Street is a 2.0 m high steel snow guard that transitions gradually to a higher 4.0 m barrier west of Loretta Avenue. The existing snow guards are being replaced with new snow guards ranging in 2.5 m to 3.0 m in height as part of the Highway 417 O-Train Bridge Replacement Project (GWP 4245-05-00).</p> <p>A noise assessment was completed as part of the Highway 417 O-Train Bridges project in accordance with the Ministry of Transportation (MTO) Noise Retrofit Policy. The noise assessment reviewed existing and potential future noise levels in this area to determine whether noise walls are warranted. The study concluded that noise barriers are not warranted as the noise attenuation from the installation of noise barriers would not provide a greater than 5 decibels (dB) reduction in noise. This noise assessment can be provided to you upon request.</p> <p>Your concerns with snow removal activities and falling snow/ice will be forwarded onto MTO Maintenance for their review.</p> <p>Staging Areas</p> <p>The areas shown on page 24 of the online Public Information Centre (PIC) materials are approximate areas only. The existing retaining wall at the rear of the Urbano complex requires rehabilitation. MTO representatives will be reaching out to the Urbano Condo Board to discuss the work in more detail and temporary impacts to the property during construction. The construction staging area currently in use between Champagne Avenue and the O-Train line is part of the rapid replacement of the Highway 417 bridges over the O-Train corridor. This is a separate rapid bridge replacement that is not part of this project.</p> <p>The replacement of the Highway 417 O-Train bridges is anticipated to be completed in the fall of 2020. The City of Ottawa is also currently undertaking the extension of the Trillium Line as part of the Stage 2 Light Rail Transit (LRT) expansion. Landscaping and reinstatement of disturbed area from Champagne Avenue to the O-train corridor will be completed by MTO's Contractor by the end of 2020. The disturbed areas on the east side of the rail corridor, including the multi-use pathway, will be completed by the City of Ottawa following completion of the Trillium Line extension works.</p> <p>For more information on the O-Train Bridge replacement project, please visit: www.highway417cpr-otrainbridges.com</p> <p>Should you have any further questions or comments, please do not hesitate to contact me.</p>	
60	February 26, 2020 / Website comment form	Public	<ul style="list-style-type: none"> • Requested to be added to project mailing list. 	<ul style="list-style-type: none"> • Contact added to mailing list for future correspondence 	No
61	February 28, 2020 / Website comment form	Public	<ul style="list-style-type: none"> • Requested to be added to project mailing list. 	<ul style="list-style-type: none"> • Contact added to mailing list for future correspondence 	No
62	June 3, 2020 / Email	Public	<ul style="list-style-type: none"> • Requested to be added to project mailing list. 	<ul style="list-style-type: none"> • Contact added to mailing list for future correspondence 	No
63	July 7, 2020 / Email	Public	<ul style="list-style-type: none"> • I have followed this project for years (It was approved notionally as early as ~2003 I believe?) and we have waited patiently, as things keep getting postponed and delayed in the construction of noise barriers for north-of-417 "not-quite-Glebe" residents.... • I am wondering if you can please confirm what the timeline is currently for completing the noise barriers on the NORTH SIDE between Bronson and Kent? 	<p>The proposed project schedule is currently being reviewed and coordinated with other ongoing projects on Highway 417. Overall construction of the project is anticipated to start in early 2021, subject to environmental approvals and Ministry priorities. Please note that the construction schedule is subject to the timing of the award of the construction contract. Furthermore, the work sequencing may vary based on the Contractor's schedule of operations in consideration of highway reconstruction and bridge replacements at Preston Street, Rochester Street, Booth Street, Bronson Avenue and Percy Street, thus it is not possible to provide specific dates for the noise barrier construction / replacement at this</p>	WSP provided a response by email on July 13, 2020

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				<p>time.</p> <p>A representative of MTO will be required to notify adjacent properties a minimum of one month in advance of construction start with a more detailed construction schedule.</p> <p>Upon completion of the Detail Design for this project, a Design and Construction Report (DCR) will be prepared and filed for a 30-day public review period. The DCR will document the Class EA process followed, including a description of the Recommended Plan, potential environmental effects, and the final mitigation plan. We have added you to the contact list to be notified directly when the DCR is filed for the 30-day public review period.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact the project team that is copied on this email.</p>	
64	July 8, 2020 / Email	Public	<ul style="list-style-type: none"> Can you please let me know when the noise Barriers will be installed on the Highway 417 South Side. I live a and received a letter in February indicating that the work would be done this summer. I hope that this project has not been cancelled. 	<p>The proposed project schedule is currently being reviewed and coordinated with other ongoing projects on Highway 417. Overall construction of the project is anticipated to start in early 2021, subject to environmental approvals and Ministry priorities. Please note that the construction schedule is subject to the timing of the award of the construction contract. Furthermore, the work sequencing may vary based on the Contractor's schedule of operations in consideration of highway reconstruction and bridge replacements at Preston Street, Rochester Street, Booth Street, Bronson Avenue and Percy Street, thus it is not possible to provide specific dates for the noise barrier construction / replacement at this time.</p> <p>A representative of MTO will be required to notify adjacent properties a minimum of one month in advance of construction start with a more detailed construction schedule.</p> <p>Upon completion of the Detail Design for this project, a Design and Construction Report (DCR) will be prepared and filed for a 30-day public review period. The DCR will document the Class EA process followed, including a description of the Recommended Plan, potential environmental effects, and the final mitigation plan. We have added you to the contact list to be notified directly when the DCR is filed for the 30-day public review period.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact the project team that is copied on this email.</p>	WSP provided a response by email on July 13, 2020
65	July 23, 2020 / Email	Public	<ul style="list-style-type: none"> I have a quick question concerning the "Highway 417 Rehabilitation and Widening from west of Maitland Avenue to east of Island Park Drive" project as my house is located in an area that the project map may or may not cover. Inquired if sound barrier in backyard will be replaced as part of this project and the timeframe. 	<p>Thankyou for your email. The noise barrier replacements adjacent to Ruskin Street have been grouped with the Ministry's bridge replacement project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor.</p> <p>The proposed project schedule is currently being reviewed and coordinated with other ongoing projects on Highway 417. Overall construction of the project is anticipated to start 2021, subject to environmental approvals and regional Ministry priorities. Please note that the construction schedule is subject to the timing of the award of the construction contract. Furthermore, the work sequencing may vary based on the Contractor's schedule of operations in consideration of highway reconstruction and bridge replacements at Preston Street, Rochester Street, Booth Street, Bronson Avenue and Percy Street, thus it is not possible to provide specific dates for the noise barrier construction / replacement at this time.</p> <p>A representative of MTO will notify adjacent properties a minimum of one month in advance of construction start with a more detailed construction schedule.</p> <p>Additional information regarding the Highway 417 Midtown Bridges Project is available on the project website at www.highway417midtownbridgesandimprovements.com.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact myself or the project team that is copied on this email.</p>	MTO provided a response by email on July 23, 2020
66	August 20, 2020 / Email	Kitchissippi Ward Councillor's office	<p>We have a resident asking about the sound barrier project. Looking back at my notes from our January, pre-Covid meeting, I see 2021 as timelines for barriers from Parkdale to Island Park Drive. I'm just wondering if that is still accurate or whether there are any Covid-related delays? Do we have any more specific timelines yet?</p>	<p>To respond your questions: the proposed project schedule is currently being coordinated with other ongoing projects on Highway 417 with construction anticipated to start in 2021, subject to environmental and funding approvals. Please note that construction of the works is scheduled to occur over five construction seasons (2021-2025) and that the work sequencing for noise barrier replacements may vary based on the Contractor's schedule of operations in consideration of other highway operational improvements and bridge replacements at Preston Street, Rochester Street, Booth Street, Bronson Avenue and Percy Street.</p>	WSP provided a response on August 24, 2020

Summary of Environmental Concerns and Comments | Highway 417 Bridge Replacements and Operational Improvements (GWP 4173-15-00)

ID	Date / Form of Contact	Name / Agency	Comments Received	How it was addressed	Response Provided
				<p>A representative of MTO will notify adjacent properties a minimum of one month in advance of construction start with a more detailed construction schedule.</p> <p>If you have any further questions, please feel free to contact the Project Team (cc'd here).</p>	
67	September 5, 2020 / Email	Public	<p>Can anyone on this string tell me who I can talk to about plans for and maintenance of queensway (Ottawa) wall and right of way between Parkdale and otrain line?</p> <p>I have specific concerns about tree cutting and the wall replacement. Please email or call any time</p>	<p>Thank you for your email. As discussed on the phone; the noise wall replacements along Young Street between the limits between Fairmont and Bayswater have been grouped with the Ministry's bridge replacement project for Preston Street, Rochester Street, Booth Street, Bronson Avenue, and Percy Street arising from the Preliminary Design and Environmental Assessment Study of the Ottawa Queensway Mid-town Bridges from Holland Avenue to O'Connor. The proposed project schedule is currently being reviewed and coordinated with other ongoing projects on Highway 417, with overall construction of the project is anticipated to start in 2021, subject to environmental approvals and Ministry priorities.</p> <p>With the removal of the existing and installation of the new noise barrier some tree/vegetation removals are required in this area. Vegetation removal will be minimized to the extent required for the Contractor to complete the work along the length of the noise wall to be replaced.</p> <p>A Design and Construction Report (DCR) will be prepared and available on the website www.highway417midtownbridgesandimprovements.com. for review at the completion of the study, which is currently planned for fall of 2020. By way of this email you will be notified directly when the Midtown Bridges DCR is available online.</p> <p>We appreciate your interest in this project. Should you have any further questions or concerns, please do not hesitate to contact the Project Team.</p>	MTO provided a response on September 8, 2020
68	September 11, 2020 / Email	Public	<p>Could you please tell me what the START DATE is for the replacement of existing noise barriers on Highway 417 on the north side of the highway, from Island Park Drive to east of Parkdale Avenue (Ottawa)? We are very much looking forward to having the rusted barriers replaced.</p>	<p>Thank you for your email. The proposed construction schedule is currently being reviewed and coordinated with other ongoing and adjacent Highway 417 projects. Overall construction of the project is anticipated to start 2021. Please note that the construction schedule is subject to the timing of the award of the construction contract and that the work sequencing may vary based on the Contractor's schedule of operations in consideration of highway reconstruction and bridge replacements at Preston Street, Rochester Street, Booth Street, Bronson Avenue and Percy Street, thus it is not possible to provide specific start dates for the noise barrier construction / replacement at this time.</p> <p>A representative of MTO will notify adjacent properties a minimum of one month in advance of construction start with a more detailed construction schedule.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact me.</p>	MTO provided a response on September 15, 2020
69	September 28, 2020 / Email	Public	<p>After reviewing the information about the Highway 417 Bridge Replacements and Operation Improvements, I noticed the following:</p> <ul style="list-style-type: none"> • Replacement of existing noise barriers on Highway 417 at the following locations: • On the north side of the highway, from Island Park Drive to east of Parkdale Avenue; • On the north side of the highway, from the Rochester westbound on-ramp, east to Bronson Avenue. <p>Please advise of the timelines on this.</p>	<p>Thank you for your question regarding the Highway 417 Midtown Bridges project. The proposed construction schedule is currently being reviewed and coordinated with other ongoing and adjacent Highway 417 projects. Overall construction of the project is anticipated to start 2021. Please note that the construction schedule is subject to the timing of the award of the construction contract and that the work sequencing may vary based on the Contractor's schedule of operations in consideration of highway reconstruction and bridge replacements at Preston Street, Rochester Street, Booth Street, Bronson Avenue and Percy Street, thus it is not possible to provide specific start dates for the noise barrier construction / replacement at this time.</p> <p>A representative of MTO will notify adjacent properties a minimum of one month in advance of construction start with a more detailed construction schedule.</p> <p>We appreciate your interest in this study. Should you have any further questions or comments, please do not hesitate to contact the Project Team copied on this email.</p>	WSP provided a response on September 30, 2020